



**STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION**

ROADWAY DESIGN DIVISION
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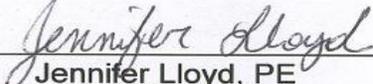
INSTRUCTIONAL BULLETIN NO. 15-04

Regarding Revised Standard Drawings

Effective May 15, 2015 letting (March 4, 2015 Turn-in), the following Standard Drawings are revised and Section V of the Design Guidelines is revised for this update

DRAWING NUMBER	CURRENT REVISION DATE	DESCRIPTION
D-PE-18A	01-06-15	18" CONCRETE ENDWALL CROSS DRAIN
D-PE-24A	01-06-15	24" CONCRETE ENDWALL CROSS DRAIN
D-PE-30A	01-06-15	30" CONCRETE ENDWALL CROSS DRAIN
RP-H-3	01-30-15	CURB RAMP AND TRUNCATED DOME SURFACE DETAIL
RP-H-4	01-30-15	PERPENDICULAR CURB RAMP
RP-H-5	01-30-15	PARALLEL CURB RAMP
RP-H-6	01-30-15	PEDESTRIAN REFUGE
RP-H-7	01-30-15	PERPENDICULAR CURB RAMP IN CURVE
RP-H-9	01-30-15	PARALEL CURB RAMP IN CURVE

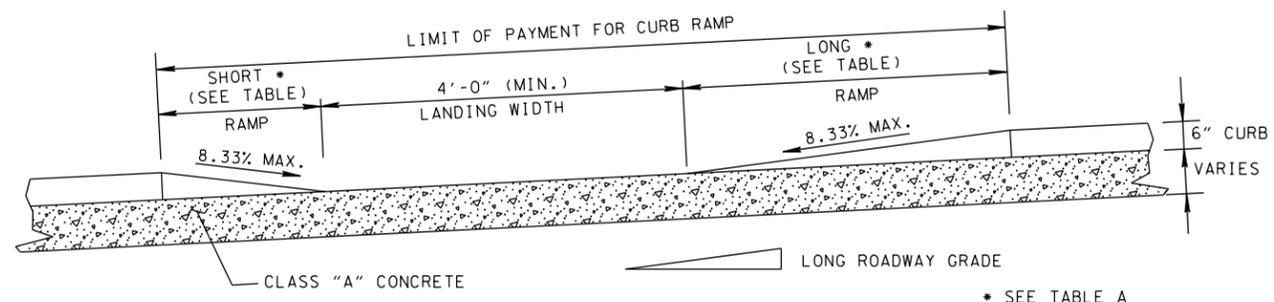
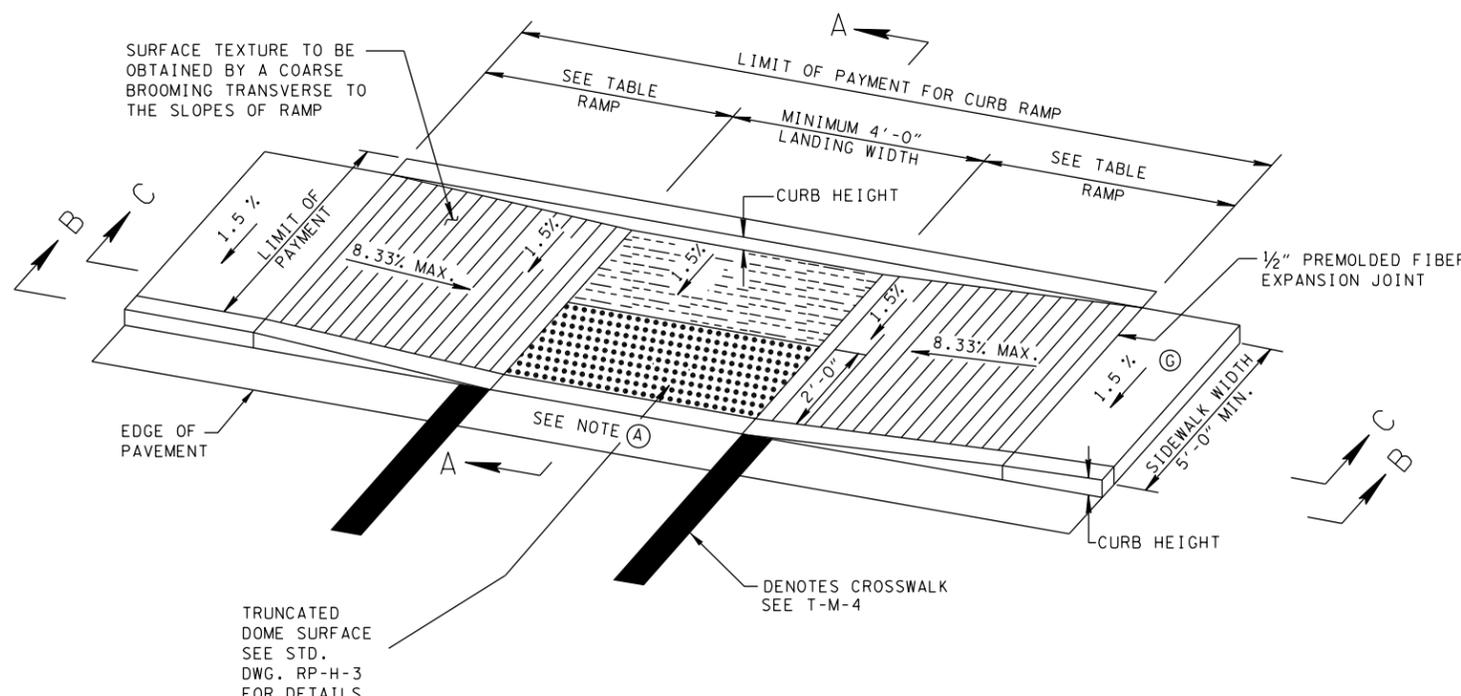
A copy of the revised standard drawings is attached.



Jennifer Lloyd, PE
Civil Engineering Director
Roadway Design Division

JCL:ARH
2/9/2015

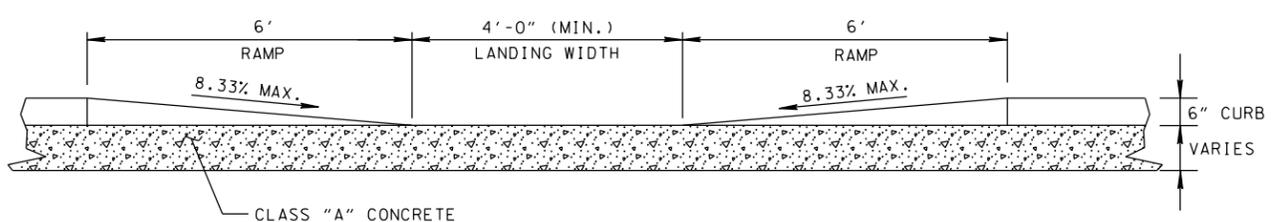
- REV. 4-13-11: ADDED FOOTNOTE ①, ADJUSTED DIMENSIONS IN RAMP DETAIL, MISC. EDITS TO DRAWING.
- REV 1-15-13: MODIFIED LANDING WIDTH, REVISED NOTES.
- REV. 6-4-13: CHANGED CROSS SLOPE TO 1.5% DESIRABLE. ADDED NOTES ③ AND ④. CHANGED ITEM DESCRIPTION.
- REV. 1-30-15: UPDATE NOTES, REMOVED PAVERS. UPDATED TABLE A.



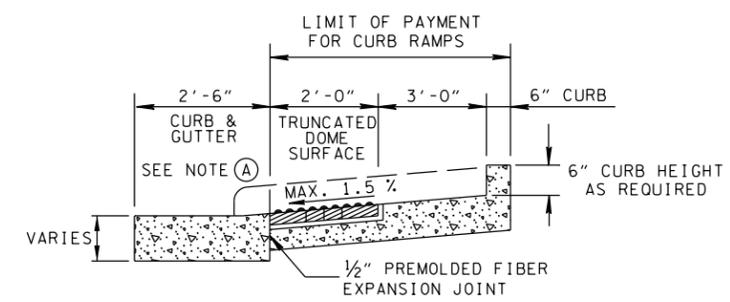
ALTERNATE SECTION B-B
PARALLEL CURB RAMP DETAIL SHOWN WITH LONGITUDINAL ROADWAY GRADE

NOTE: MODIFICATIONS MAY BE REQUIRED FOR LONGITUDINAL ROADWAY GRADES STEEPER THAN 5%. ENGINEER SHOULD BE NOTIFIED FOR ASSESMENT IF THE CURB RAMP SIDE FLARES EXCEED 15' IN LENGTH DUE TO THE LONGITUDINAL ROADWAY GRADE.

PARALLEL CURB RAMP DETAIL
DIMENSIONS SHOWN ABOVE FOR 0% LONGITUDINAL ROADWAY GRADE



SECTION B-B
DIMENSIONS SHOWN ABOVE FOR 0% LONGITUDINAL ROADWAY GRADE



SECTION A-A

LONGITUDINAL ROADWAY GRADE	LONG	SHORT	PAY AREA S.F.
5 %	15'	3'9"	114
4 %	11'6"	4'1"	98
3 %	9'5"	4'5"	90
2 %	7'11"	4'10"	84
1 %	6'10"	5'5"	82
0 %	6'0"	6'0"	

① BASED ON 5' SIDEWALK WIDTH

- GENERAL NOTES**
- IT IS RECOMMEND TO USE WHEN SIDEWALK WIDTH IS LESS THAN 10 FEET AND CURB RAMP IS LOCATED ALONG TANGENT SIDEWALK SECTION OF ROADWAY.
- (A) CURB SHALL BE LOWERED ACROSS ENTIRE WIDTH OF RAMP THE FIRST TWO FEET OF RAMP MUST CONSIST OF A TRUNCATED DOMED SURFACE. RAMPS SHALL INCLUDE THE TRUNCATED DOME SURFACE TO PROVIDE A DETECTABLE WARNING FOR VISUALLY IMPAIRED PEDESTRIANS PARALLEL RAMPS DO NOT NEED GUTTER SLOPE CORRECTION.
 - (B) THE COST OF THE LOWERED CURB AND GUTTER TO BE INCLUDED IN THE PRICE OF ITEM NO. 702-01, CONCRETE CURB OR ITEM NO. 702-03, CONCRETE COMBINED CURB & GUTTER.
 - (C) DESIGN/CONSTRUCTION MODIFICATIONS MAY BE REQUIRED FOR CURB RAMPS TO BE INSTALLED ALONG A ROADWAY WITH LONGITUDINAL GRADES EXCEEDING FIVE PERCENT.
 - (D) ALL COST OF INSTALLING CURB RAMPS IN NEWLY CONSTRUCTED SIDEWALK AREAS SHALL BE BID FOR UNDER THE FOLLOWING PAY ITEM:
701-02.03 CONCRETE CURB RAMP PER SQUARE FOOT.
 - (E) IF PEDESTRIAN SIGNAL IS PROPOSED SEE TDOT TRAFFIC DESIGN MANUAL FOR PLACEMENT.
 - (F) IF MAILBOXES ARE REMOVED DURING INSTALLATION OF THE RAMP PROVIDE A 12" X 12" OPENING BEHIND THE CURB (SEE RP-S-7).
 - (G) DESIRABLE CROSS SLOPE IS 1.5%, ABSOLUTE MAXIMUM IS 2%.
 - (H) SEE NOTES ① AND ② ON RP-H-3.

□ MINOR REVISION -- FHWA APPROVAL NOT REQUIRED.

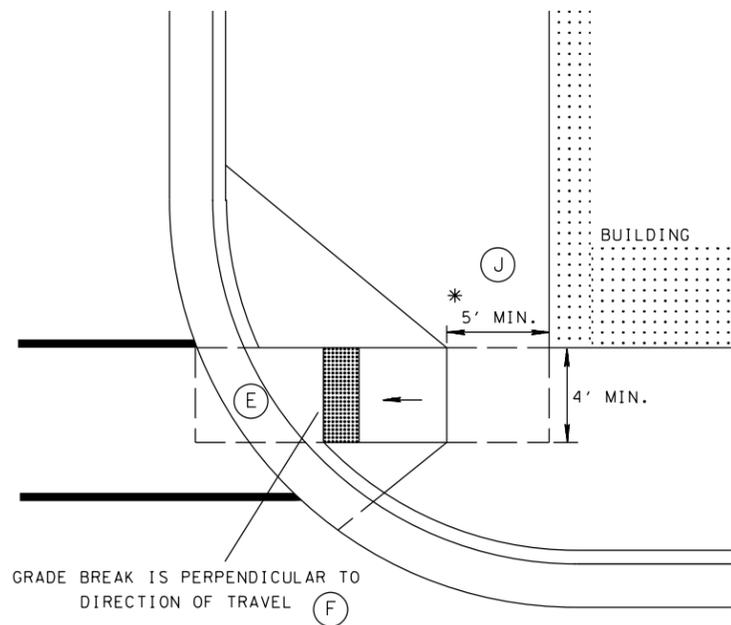
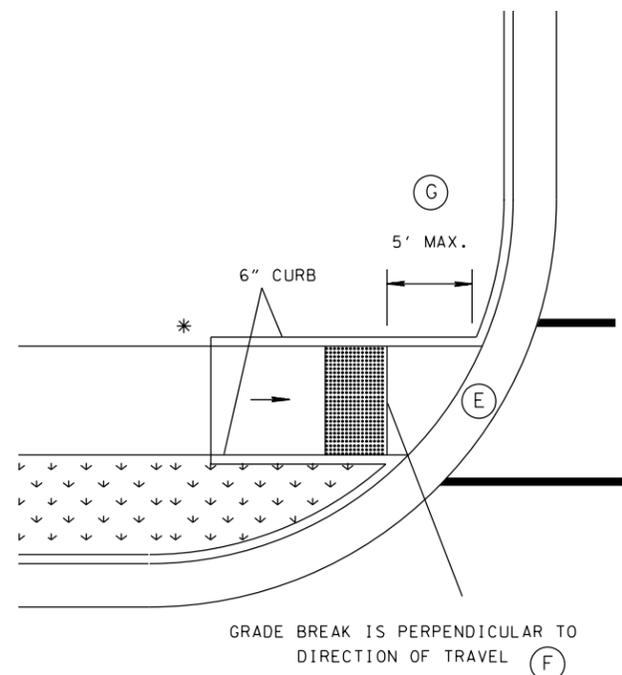
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PARALLEL CURB RAMP

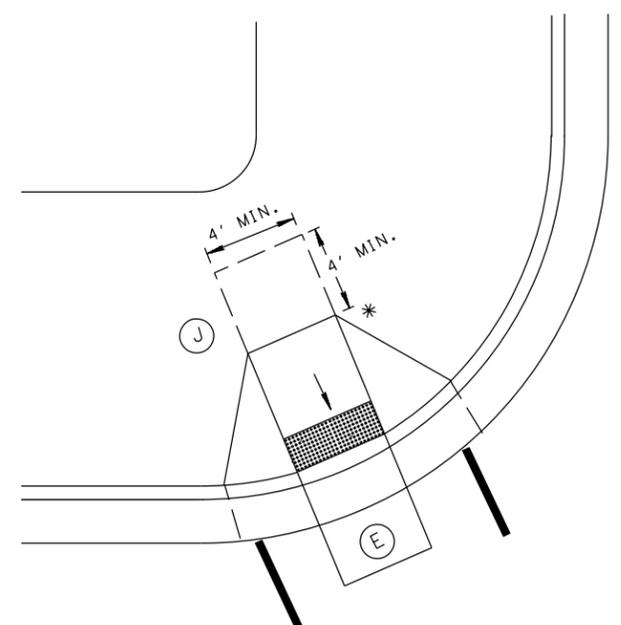
1-15-07 RP-H-5

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- REV. 4-13-11: REVISED TABLE DIMENSIONS, ADDED NOTE (I), AND ADDED GUTTER TO CROSSWALK INTERSECTION DIMENSION.
- REV. 5-8-13: REVISED TITLE FOR TERMINOLOGY.
- REV. 6-4-13: REVISED NOTE (C), CHANGED TITLE.
- REV. 1-30-15: REMOVED ALTERNATE, ADDED GENERAL NOTE (E), (F), (G), (H), (I), AND (J). ADDED GRADE BREAKS DETAILS.



GRADE BREAKS
OPTION TO PERPENDICULAR CURB RAMP



* DENOTES: PEDESTRIAN PUSHBUTTON LOCATION FOR SIGNALIZED INTERSECTIONS

PERPENDICULAR CURB RAMP
SEE RP-H-4 FOR DETAILS

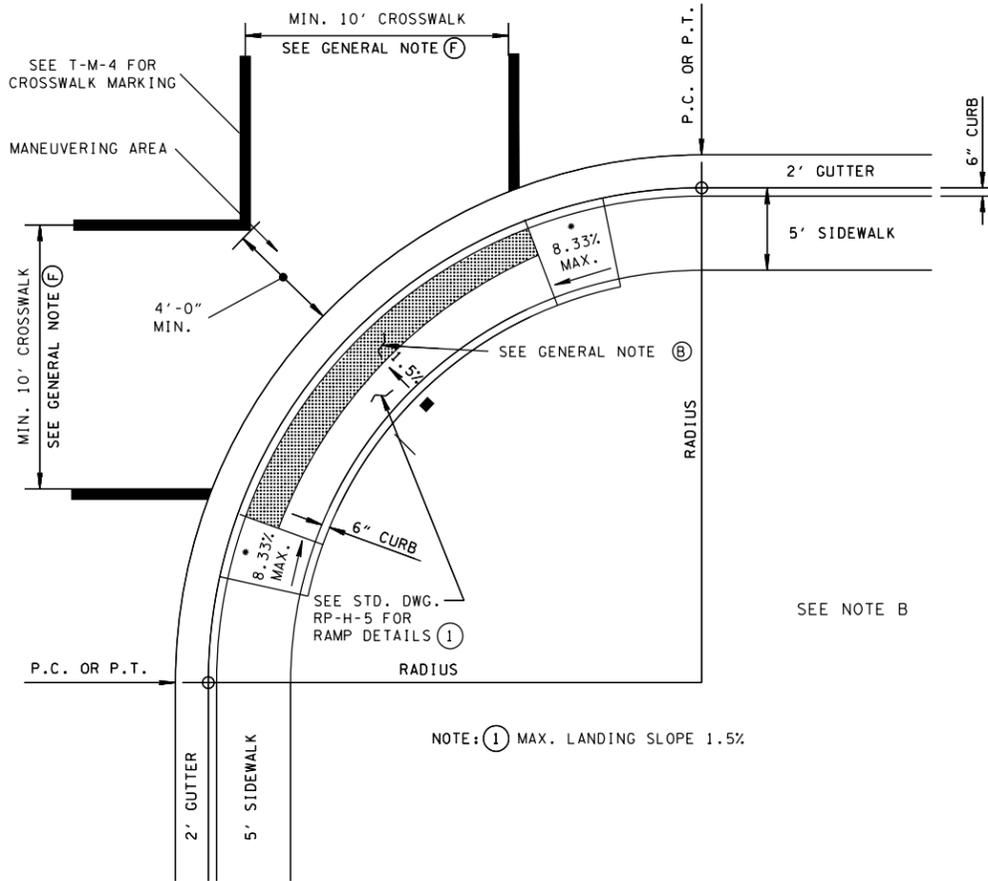
GENERAL NOTES	
(A)	FOR SIGNALIZED INTERSECTIONS THAT REQUIRE PEDESTRIAN SIGNAL PUSH BUTTONS, SEE TDOT TRAFFIC DESIGN MANUAL FOR PLACEMENT DETAILS.
(B)	SEE STANDARD DRAWING RP-H-3 FOR TRUNCATED DOMED SURFACE DETAILS AND GUTTER SLOPE AT RAMP DETAILS.
(C)	SIDEWALK WIDTH SHALL NOT INCLUDE 6" CONCRETE CURB.
(D)	GRATES FOR STORM DRAINS SHALL NOT BE PLACED IN THE ACCESSIBLE ROUTE.
(E)	BEYOND THE BOTTOM GRADE BREAK, A CLEAR SPACE 4' MIN. BY 4' MIN. SHALL BE PROVIDED WITHIN THE WIDTH OF THE PEDESTRIAN STREET CROSSING AND WHOLLY OUTSIDE THE PARALLEL VEHICLE TRAVEL LANE.
(F)	GRADE BREAKS AT THE TOP AND BOTTOM OF CURB RAMP RUNS SHALL BE PERPENDICULAR TO THE DIRECTION OF THE RAMP RUN. GRADE BREAKS SHALL NOT BE PERMITTED ON THE SURFACE OF RAMP RUNS AND TURNING SPACES. SURFACE SLOPES THAT MEET AT GRADE BREAKS SHALL BE FLUSH.
(G)	WHERE THE END OF BOTTOM GRADE BREAK ARE BEHIND THE BACK OF CURB AND THE DISTANCE FROM EITHER END OF THE BOTTOM GRADE BREAK TO THE BACK OF CURB IS 5' OR LESS, DETECTABLE WARNING SURFACES SHALL BE PLACED ON THE RAMP RUN WITHIN ONE DOME SPACING OF THE BOTTOM GRADE BREAK.
(H)	WHERE PEDESTRIAN SIGNALS ARE PROVIDED AT PEDESTRIAN STREET CROSSINGS, THEY SHALL INCLUDE ACCESSIBLE PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTONS COMPLYING WITH SECTIONS 4E.08 THROUGH 4E.13 OF THE MUTCD.
(I)	WHERE THE TURNING SPACE IS CONSTRAINED AT THE BACK-OF-SIDEWALK, THE TURNING SPACE SHALL BE 4' MIN. BY 5' MIN. THE 5' DIMENSION SHALL BE PROVIDED IN THE DIRECTION OF THE RAMP RUN.
(J)	SEE RP-H-4 FOR PERPENDICULAR RAMP DETAILS.
(K)	PAYMENT FOR NEW RAMPS SHALL BE UNDER ITEM NUMBER: 701-02.03 CONCRETE CURB RAMP PER SF 701-02.01 CONCRETE CURB RAMP (RETROFITS) PER SF COST TO INCLUDE ALL MATERIALS AND COUNTER SLOPE CORRECTION OF THE GUTTER (INCLUDING TRUNCATED DOMES) LABOR AND EQUIPMENT.

□ MINOR REVISION -- FHWA APPROVAL NOT REQUIRED.

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PERPENDICULAR
CURB RAMP
IN CURVE

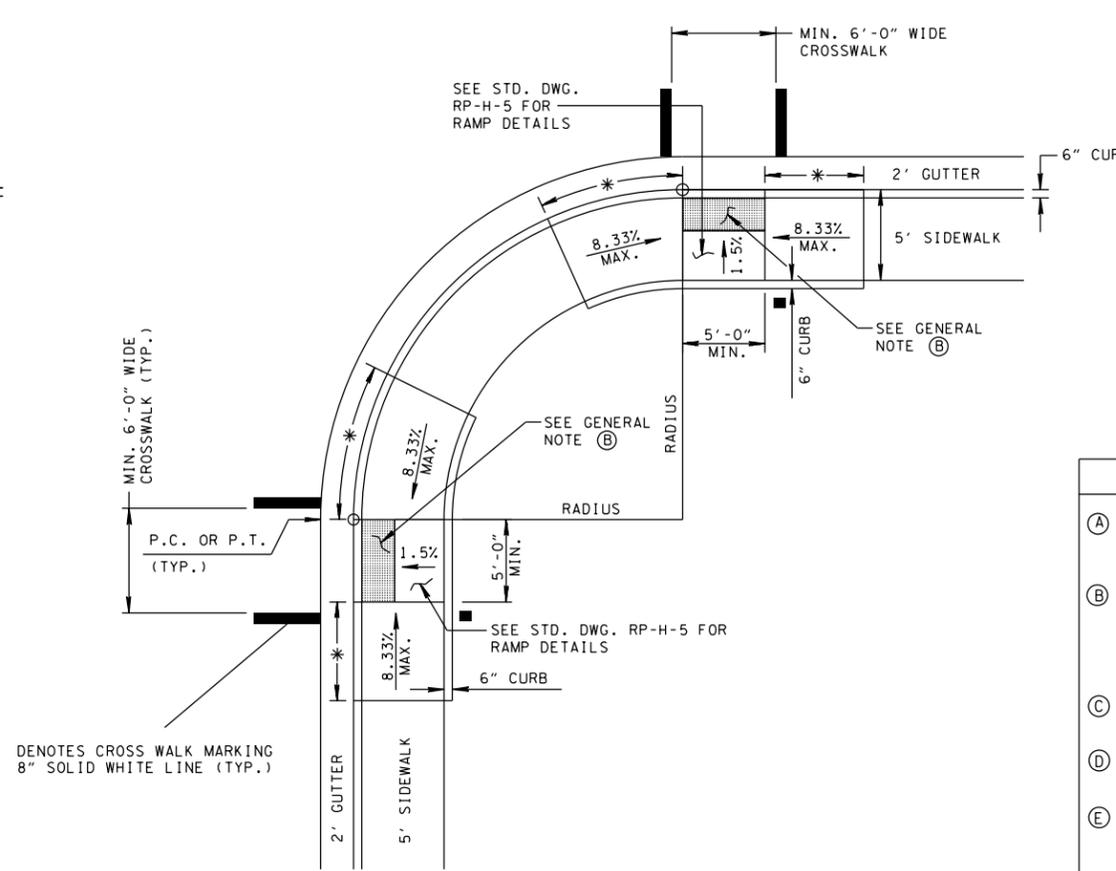
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LOWERED CORNER RAMP

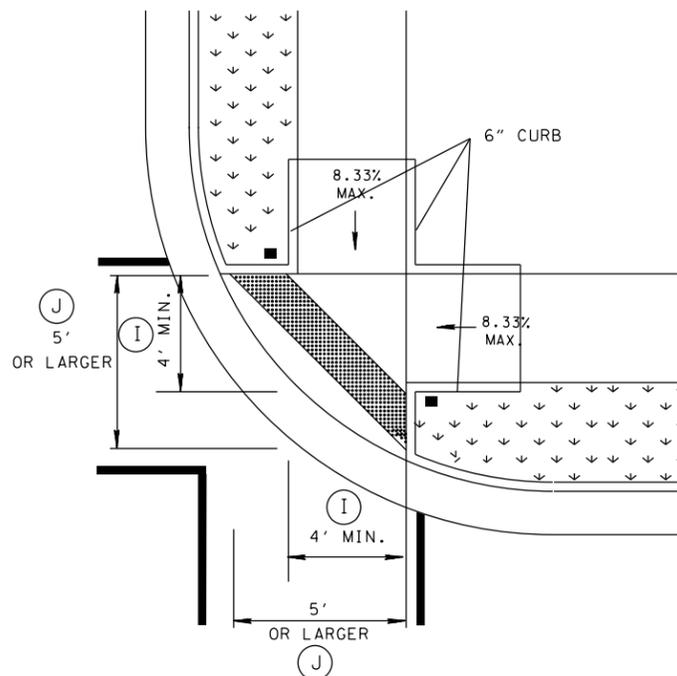
(RAMP IN RADIUS)

* DIMENSION VARIES RELATIVE TO LONGITUDINAL ROADWAY GRADE



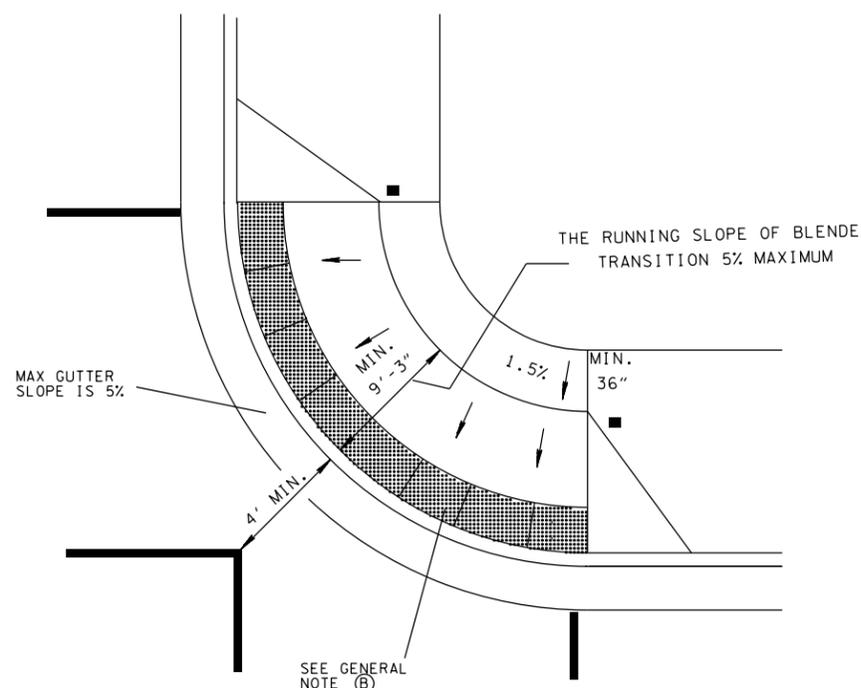
PARALLEL CURB RAMPS
OUTSIDE RADIUS

* DIMENSION VARIES RELATIVE TO LONGITUDINAL ROADWAY GRADE



PARALLEL CURB RAMP

■ DENOTES:
PEDESTRIAN
PUSHBUTTON
LOCATION FOR
SIGNALIZED
INTERSECTIONS



BLENDED TRANSITION

BLENDED TRANSITIONS ARE RAISED PEDESTRIAN STREET CROSSINGS, DEPRESSED CORNERS, OR SIMILAR CONNECTIONS BETWEEN PEDESTRIAN ACCESS ROUTES AT THE LEVEL OF THE SIDEWALK AND THE LEVEL OF THE PEDESTRIAN STREET CROSSING THAT HAVE A GRADE OF 5% OR LESS. BLENDED TRANSITIONS ARE SUITABLE FOR A RANGE OF SIDEWALK CONDITIONS.

- REV. 4-13-11: ADJUSTED CROSSWALK MARKINGS, ADDED NOTE ① REVISED TABLE DIMENSIONS, ADDED GUTTER TO CROSSWALK INTERSECT DIMENSION, OTHER MISC. EDITS TO DRAWINGS.
- REV. 5-8-13: REVISED TITLE FOR TERMINOLOGY.
- REV. 6-4-13: ADDED NOTE ②, MODIFIED NOTE ③, CHANGED SHEET NAME.
- REV. 1-30-15: REMOVED TYPE 4 REVISED NOTE ④, ADDED NOTES ⑤, ⑥ AND ⑦, REMOVED TABLE A, ADDED NEW DETAILS.

GENERAL NOTES

- (A) FOR SIGNALIZED INTERSECTIONS THAT REQUIRE PEDESTRIAN SIGNAL PUSH BUTTONS, SEE TDOT TRAFFIC DESIGN MANUAL FOR PLACEMENT DETAILS.
- (B) SEE STANDARD DRAWING RP-H-3 FOR TRUNCATED DOME SURFACE AND GUTTER SLOPE AT RAMP DETAILS. DOME SPACING. WHERE THE TRUNCATED DOMES ARE ARRAYED RADIALLY, THEY MAY DIFFER IN CENTER-TO-CENTER SPACING. REFER TO RP-H-3 FOR SPECIFIED DOME SPACING RANGES.
- (C) 5'-0" SIDEWALK WIDTH SHALL NOT INCLUDE 6" CONCRETE CURB.
- (D) GRATES FOR STORM DRAINS SHALL NOT BE PLACED IN THE CROSSWALK OR IN FRONT OF THE CURB RAMP.
- (E) BEYOND THE BOTTOM GRADE BREAK, A CLEAR SPACE 4' MIN. BY 4' MIN. SHALL BE PROVIDED WITHIN THE WIDTH OF THE PEDESTRIAN STREET CROSSING AND WHOLLY OUTSIDE THE PARALLEL VEHICLE TRAVEL LANE.
- (F) LOCATE CROSSWALK(S) SUCH THAT 4' MINIMUM MANEUVERING AREA IS LOCATED AT THE TOE OF THE RAMP IS LOCATED ENTIRELY WITHIN THE CROSSWALK.
- (G) DESIREABLE CROSS SLOPE IS 1.5 %, ABSOLUTE MAXIMUM IS 2.0%.
- (H) TO BE USED WHERE SIDEWALK WIDTHS ARE LESS THAN 10 FEET WIDE RAMP IS LOCATED ANYWHERE WITHIN A CURVED SECTION OF SIDEWALK.
- (I) BEYOND THE BOTTOM GRADE BREAK, A CLEAR SPACE 4' MIN. BY 4' MIN. SHALL BE PROVIDED WITHIN THE WIDTH OF THE PEDESTRIAN STREET CROSSING AND WHOLLY OUTSIDE THE PARALLEL VEHICLE TRAVEL LANE.
- (J) WHERE THE ENDS OF BOTTOM GRADE BREAK ARE BEHIND THE BACK OF CURB AND THE DISTANCE FROM EITHER END OF THE BOTTOM GRADE BREAK TO THE BACK OF CURB IS MORE THAN 5', DETECTABLE WARNING SURFACES SHALL BE PLACED ON THE LOWER LANDING AT THE BACK OF CURB.
- (K) COPY OF CURB RAMPS TO INCLUDE ALL MATERIALS, LABOR AND EQUIPMENT (INCLUDING TRUNCATED DOMES) PAYMENT FOR NEW RAMPS SHALL BE UNDER ITEM NUMBER 701-02.03 CONCRETE CURB RAMP PER SF. PAYMENT FOR RECONSTRUCTED CURB RAMPS SHALL BE UNDER ITEM NUMBER 701-02.01 CONCRETE CURB RAMP (RETROFITS) PER SF.
- (L) SEE RP-H-5 FOR ALL PARALLEL RAMP DETAILS NOT SHOWN.

□ MINOR REVISION -- FHWA APPROVAL NOT REQUIRED.

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PARALLEL CURB RAMP
IN CURVE

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